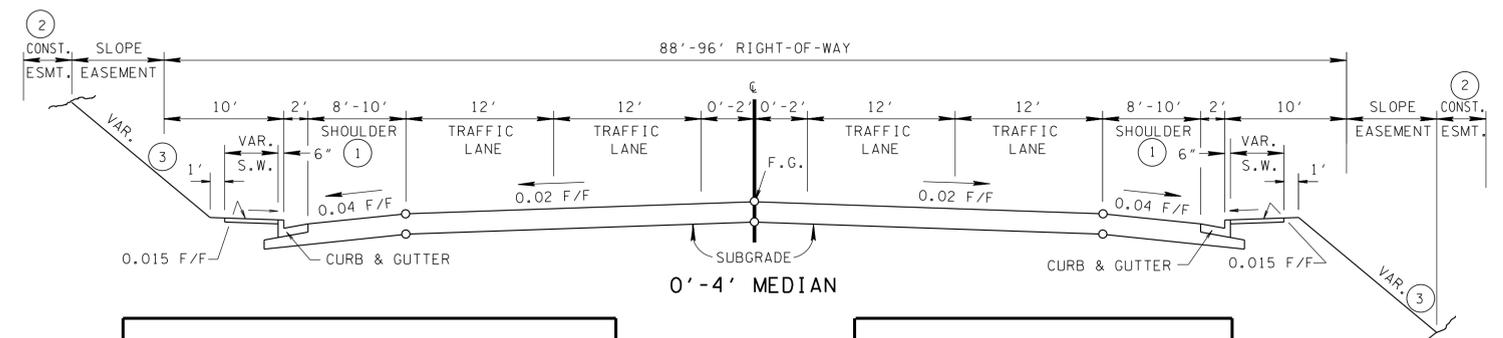
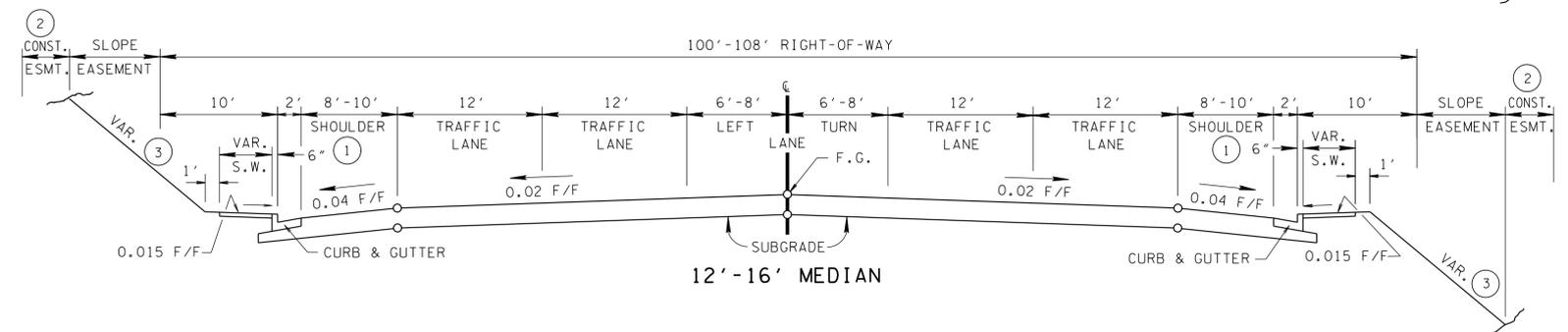
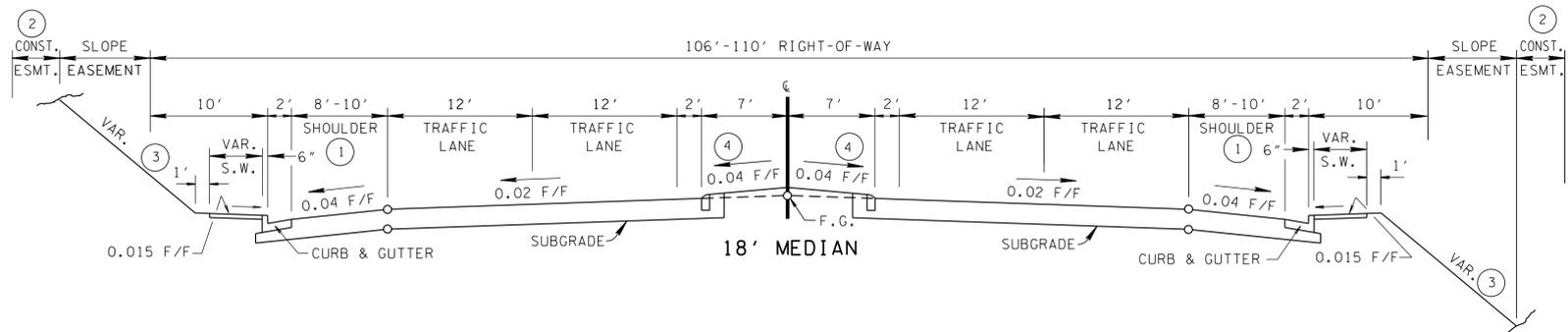
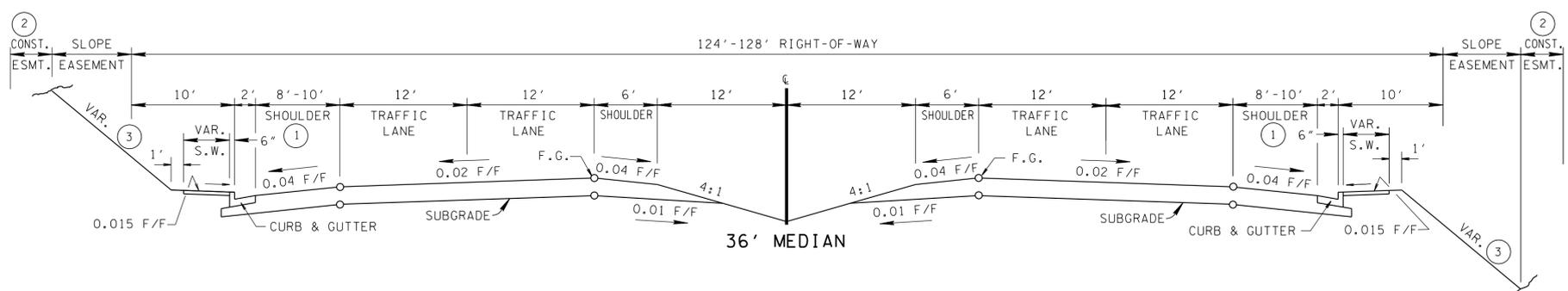


REV. 10-15-02: NEW SHEET. REPLACES RD-TS-6.
 □ REV. 7-31-13: REVISED SIDEWALK WIDTH.
 □ REV. 1-25-16: REVISED GENERAL NOTES.
 REV. 10-10-16: REVISED GENERAL NOTES. CLARIFIED SIDEWALK DIMENSION.



GENERAL NOTES

DESIGN SPEED
 THESE SECTIONS ARE FOR 45 MILES PER HOUR OR LESS. IF HIGHER SPEED IS PROPOSED USE RD01-TS-6B.

ALIGNMENT
 SEE S-PL-6.

SUPERELEVATION AND MEDIAN BARRIERS
 SEE APPROPRIATE STANDARD DRAWING IN THE RD01-SE-SERIES AND S-SSMB-SERIES.

SHOULDER
 ① IF SHOULDERS LESS THAN 8', USE RD01-TS-6A.
 8" MIN. SHOULDER IS REQUIRED FOR VEHICLE EMERGENCIES AND TO PROVIDE PROPER HORIZONTAL OFFSET TO PEDESTRIAN FACILITIES. UNDER NO CIRCUMSTANCES SHALL THE SHOULDER BE CONSIDERED TO FACILITATE PEDESTRIAN ACCESS. LOCATIONS WHERE EXISTING ROADWAY IS LACKING MIN 8' SHOULDER WIDTH (3R PROJECTS-RESURFACING, RETROFITTING, AND RECONSTRUCTION), MIN 4' SHALL BE PROVIDED. IN SOME INSTANCES, ADJUSTMENT TO LANE WIDTH MAY BE REQUIRED.

CONSTRUCTION EASEMENT
 ② 10' MINIMUM DESIRABLE.

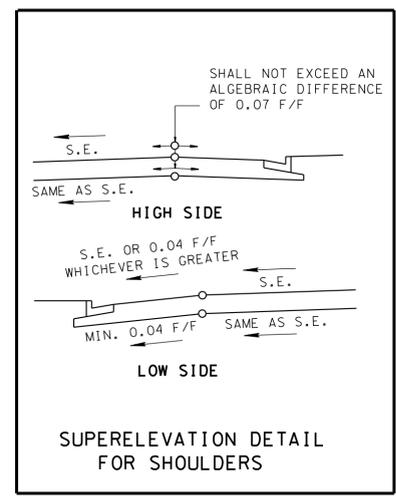
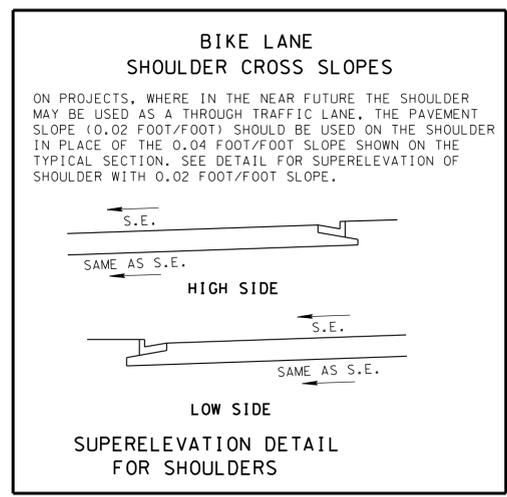
SLOPES
 ③ ON URBAN PROJECTS, THE BACKSLOPE AND FORESLOPE DESIGN WILL VARY FROM PROJECT TO PROJECT; AS A GENERAL RULE USE THE FOLLOWING:
 3:1 SLOPES OR FLATTER ARE DESIRABLE AND ARE THE MAXIMUM IN REGION IV. 2:1 SLOPES ARE APPLICABLE IN AREAS WHERE RIGHT-OF-WAY RESTRICTIONS OR COST WARRANTS A STEEPER THAN 3:1 SLOPE.

CURBS
 ④ MEDIAN CURBS WILL BE 6" SLOPING CURBS. OUTSIDE CURBS WILL BE 4" SLOPING CURBS (SEE RP-MC-1). 6" VERTICAL CURBS MAY BE USED IN URBAN ZONES.

SIDEWALKS AND BIKE LANES
 SIDEWALK WIDTH SHALL NOT INCLUDE THE 6" WIDTH OF PROPOSED CURB, SIDEWALK SHALL BE A MINIMUM OF 5' WIDE. REFER TO RP-H-SERIES FOR CURB RAMP DETAILS. IF BIKE LANE IS PROPOSED, BIKE LANE SHALL BE PLACED AS FAR AWAY AS POSSIBLE FROM THE EDGE OF TRAVELED WAY. SEE BIKE LANE BUFFER DETAILS ON T-M-12.

PARKING LANES
 FOR SPECIFIC CONDITIONS NOT COVERED ON THIS SHEET, REFERENCE SHOULD BE MADE TO "A POLICY ON GEOMETRIC DESIGN OF HIGHWAYS AND STREETS," AASHTO, 2011 PAGE NUMBERS 4-72 THROUGH 4-74, 5-14, 6-14, 7-34, AND TO THE CURRENT MUTCD.

3R PROJECTS-RESURFACING, RETROFITTING, AND RECONSTRUCTION
 LOCATIONS WHERE EXISTING ROADWAY IS LACKING MIN 8' SHOULDER WIDTH, MIN 4' SHALL BE PROVIDED. IN SOME INSTANCES, ADJUSTMENT TO LANE WIDTH MAY BE REQUIRED. IF MIN 4' SHOULDER CANNOT BE ACHIEVED DUE TO THE ROW LIMITATIONS, UTILITY CONFLICTS, ETC., THEN ADDITIONAL MITIGATION STRATEGIES SUCH AS REDUCING DESIGN SPEED OR GROUND MOUNTED ADVANCED WARNING SIGNS SHALL BE CONSIDERED.



□ MINOR REVISION -- FHWA APPROVAL NOT REQUIRED.

STATE OF TENNESSEE
 DEPARTMENT OF TRANSPORTATION

TYPICAL CURB AND GUTTER SECTIONS WITH SHOULDER

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